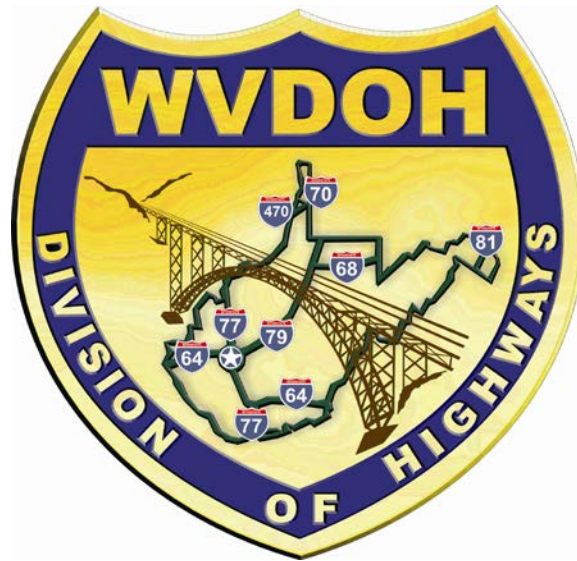


Informational Workshop Public Meeting

Henlawson Thru Truss Bridge



**WV Department of Transportation
Division of Highways
In cooperation with the Federal Highway
Administration**

**State Project S323-12/4-0.05
Federal Project BR-0124(003)D**

**WV Department of Veterans Assistance Field Office
Henlawson, WV
Logan County
Tuesday, March 6, 2012
4:00PM to 7:00PM**

Project Description

The Henlawson Thru Truss Bridge was constructed in 1925 in Fayette County (over the Gauley River) with trusses fabricated by Vincennes Bridge Company of Vincennes, Indiana and built by Standard Engineering and Contracting Company of Toledo, Ohio. The trusses were dismantled and moved to Henlawson in 1953. The bridge was completely renovated in 1985 by Turman Construction Company of Barboursville, WV. The bridge consists of five simple spans including a simple-span steel thru truss, two steel simple-span pony trusses, and two simple-span steel girder spans. Also, there is a 3'9" wide sidewalk on the upstream side of the structure. The overall length of the bridge is 416'5 ¼" face to face of backwalls. Signage on the bridge approaches list the name of the bridge as *Robert T. "Bob" Carter Vietnam Veteran Bridge*.

The existing bridge provides access to and from the town of Henlawson which has no alternative accesses to WV 10. The bridge serves approximately 200 homes and a population of approximately 442 (per 2010 census).



Purpose and Need

The purpose of the project is to find the most suitable and economical location for a new bridge in Henlawson, WV. The new bridge must meet current design standards including suitable weight limits to efficiently serve the residential, commercial and business community.

Construction Schedule

The bridge is currently scheduled for construction in the summer of 2013.

Alternatives

- **Alternative One**- Replaces the bridge on a new alignment approximately 475 feet upstream from the existing bridge. This alternative proposes a two span bridge approximately 300 feet in length with a 31 foot clear width. Approximately 85 feet of new approach work will be necessary north of the proposed bridge and approximately 270 feet of new approach work to the south. The new alignment would require a new signalized railroad crossing to be constructed. Right of way involvement will be significant. Permanent right of way will be required for the approaches and new bridge, impacting one unoccupied commercial property along WV 10 and possibly three residential properties along County Route 701/91 and coordination with CSX Railroad. Traffic will be maintained on the existing bridge during construction. **Total cost of construction is \$6,586,000.**
- **Alternative Two**-Replaces the bridge on new alignment upstream from the existing bridge. The proposed bridge is skewed and meets the existing County Route 12/4 alignment avoiding the US Post Office and utilizing the current at-grade railroad crossing. This alternative proposes a two span bridge approximately 350 feet in length with a 31 foot clear width. Approximately 100 feet of new approach work will be necessary north of the proposed bridge and approximately 190 feet of new approach work to the south. Right of way involvement will be moderate. It will include permanent right of way takes for County Route 12/4, impacting one unoccupied commercial property along WV 10 and one residential property along County Route 701/91. Traffic will be maintained on the existing bridge during construction. **Total cost of construction is \$7,441,000.**
- **Alternative Three (Preferred Alternative)**- Replaces the bridge adjacent to the existing bridge. The proposed structure will be located approximately 44 feet upstream of the existing bridge. The total length of construction will be about 700 feet which includes a 350 foot bridge and a new signalized railroad crossing. The new bridge will have two spans with one stub abutment and one full height abutment. Two causeways will be needed during construction, one upstream of the proposed bridge and one downstream of the existing bridge. Right of way involvement will be significant. The alignment will require additional right of way and acquisition of two vacant lots and one residential home, and coordination with CSX Railroad. Traffic will be maintained on the existing bridge during construction. **Total cost of construction is \$7,410,000.**
- **Alternative Four**- Replaces the bridge on a new alignment downstream from the existing bridge. To provide a causeway between the existing and proposed bridge, the proposed centerline will be approximately 70 feet downstream from the existing centerline. This alternative proposes a two span bridge approximately 350 feet in length with a 31 foot clear width. Approximately 100 feet of new approach work will be necessary north of the proposed bridge and approximately 250 feet of new approach work to the south. The new alignment will require a new signalized railroad crossing. The alignment will require additional right of way, impacting one commercial property along WV 10, acquisition of two residential homes along County Route 701/90, and coordination with

CSX Railroad. Traffic will be maintained on the existing bridge during construction. **Total cost of construction is \$8,806,000.**

- **Alternative Five**- Replaces the bridge on new alignment approximately 500 feet downstream from the existing bridge. This alternative proposes a two span approximately 350 feet in length with a 31 foot clear width. The total length of construction will be about 700 feet which includes a 350 foot bridge and a new signalized railroad crossing. The new bridge will have two spans with one stub abutment and one full height abutment. Right of way involvement will be significant. The alignment will require additional right of way, impacting one commercial property along WV 10, acquisition of three residential homes along County Route 701/90, and coordination with CSX Railroad. Traffic will be maintained on the existing bridge during construction. **Total cost of construction is 7,450,000.**
- **Alternative Six**- This alternative renovates the existing bridge. The rehabilitation will include the following elements: rehabilitate the abutment, truss, and floor system. Also the deck will be replaced along with cleaning and painting the bridge. Traffic will be maintained on the existing bridge during rehabilitation, with lane at a time construction and intermittent closure. **Total cost for renovation is \$6,593,000.**
- **No-Build Option**- This alternative will eventually lead to the closure of the bridge. No feasible detour route exists for the bridge closure that will occur.

Informational Meeting

The purpose of this informational workshop public meeting is to afford participants an opportunity to ask questions and state their views and opinions on the project. Your **comments are important**: comment sheets will be provided at the workshop; your comments can be dropped in a comment box at the workshop, or mailed to:

Gregory Bailey, P.E., Director, Engineering Division
West Virginia Division of Highways
Capital Complex Building Five, Room 317
1900 Kanawha Boulevard East
Charleston, West Virginia 25305-0430

Any additional questions or comments can be sent to Sondra Mullins (Sondra.L.Mullins@wv.gov) or visit the WVDOH Website at www.transportation.wv.gov for project information and the opportunity to comment on the project. **Comments are due by Friday, April 6, 2012.**

This project will be processed as a Categorical Exclusion to clear the Environmental (NEPA) requirement.

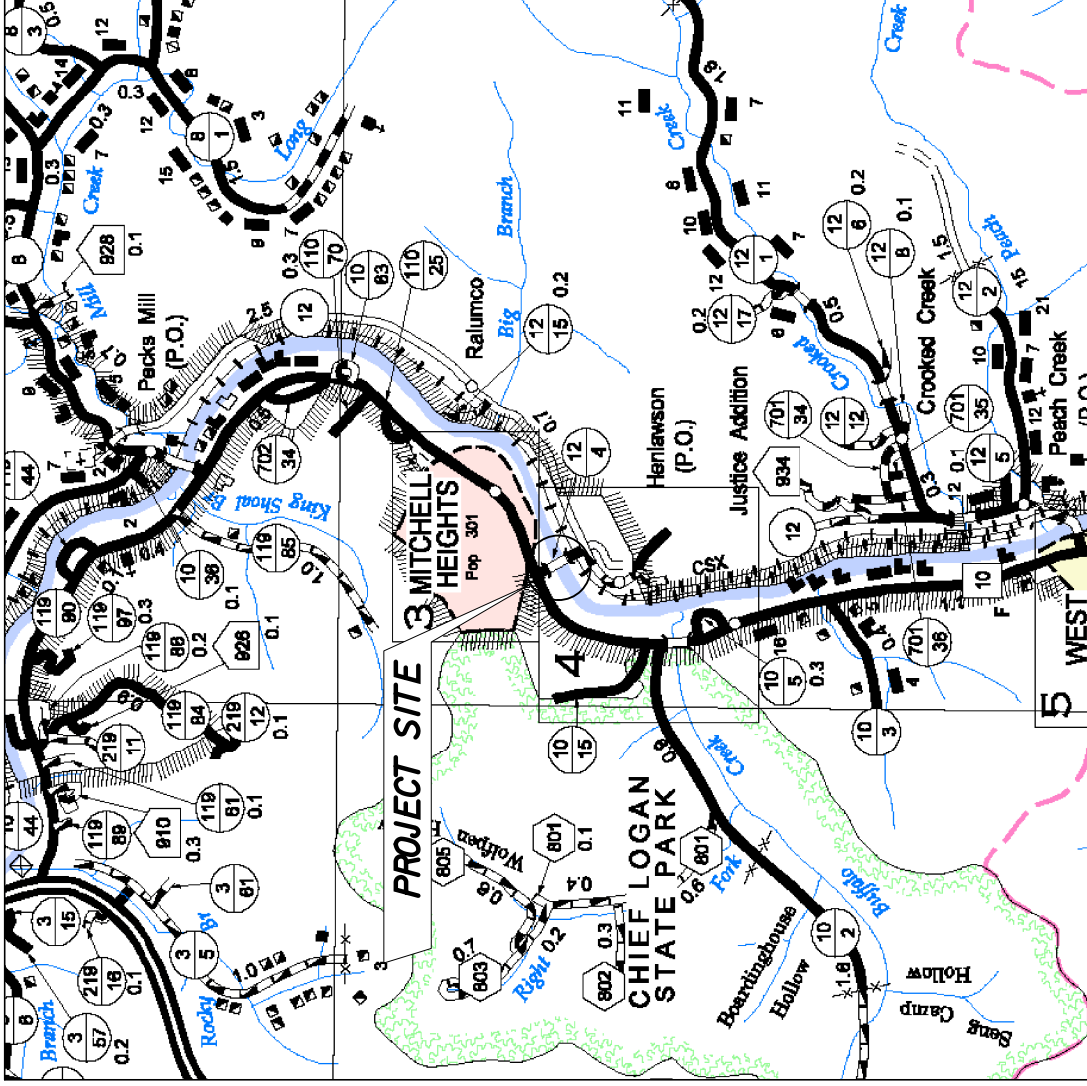
BRIDGE REPLACEMENT STUDY

HENLAWSON THRU TRUSS

STATE PROJECT NO. S323-12/4-0.05

FEDERAL PROJECT NO. BR-0124(003)D

LOGAN COUNTY



WEST VIRGINIA DEPARTMENT OF TRANSPORTATION

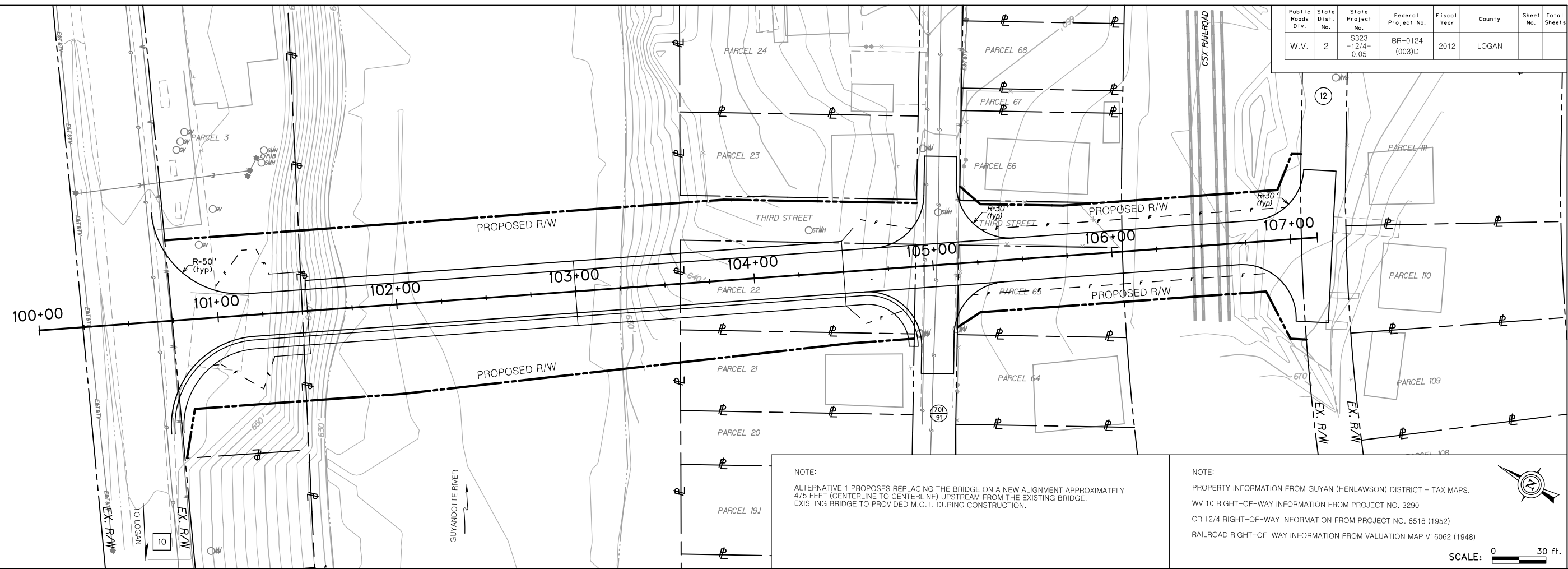
DIVISION OF HIGHWAYS

ENGINEERING DIVISION

FEBRUARY 2012

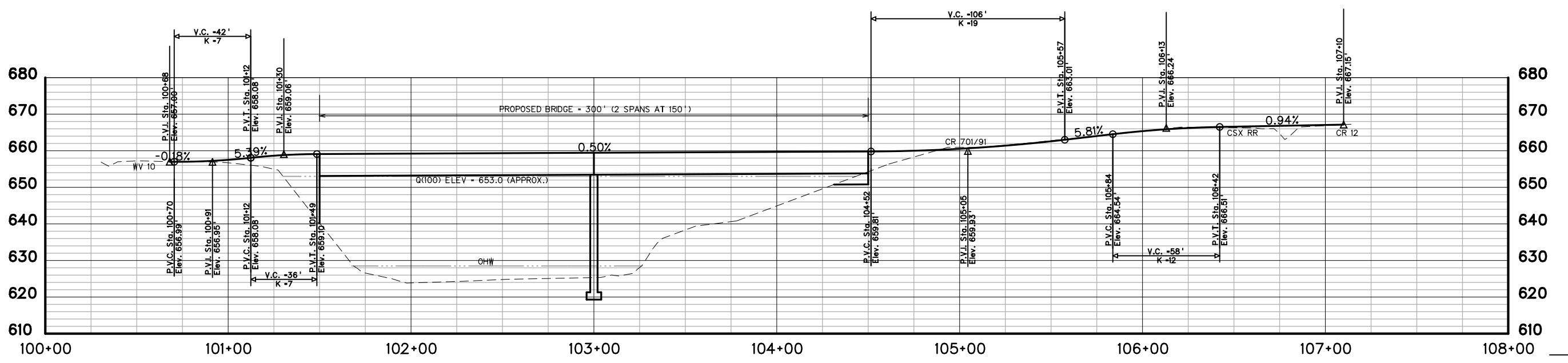
ALTERNATIVES							
Item	1	2	3 Preferred	4	5	6	7
	New Alignment 475' Upstream	New Alignment 150' Upstream of Existing Bridge	New Alignment 44' Upstream of the Existing Bridge	New Alignment 70' Downstream	New Alignment 350' Downstream	Renovate Existing Bridge	No Build Option
Roadway Length (ft)	355	290	350	350	350	n/a	n/a
Bridge Length (ft)	300	350	350	350	350	n/a	n/a
Total Length (ft)	655	640	700	700	700	n/a	n/a
Commerical Properties Required	1	1	3	1	0	0	n/a
Residents Required	3	1	1	2	3	0	n/a
Construction Cost	\$4,801,000	\$581,100	\$5,965,000	\$6,336,000	\$5,695,000	\$6,293,000	n/a
Engineering Cost	\$600,000	\$600,000	\$600,000	\$600,000	\$600,000	\$300,000	n/a
Railroad	\$350,000	\$80,000	\$350,000	\$350,000	\$350,000	n/a	n/a
Environmental (Site Clean-up)	\$300,000	n/a	n/a	\$160,000	n/a	n/a	n/a
Right-of-Way Cost	\$485,000	\$860,000	\$430,000	\$1,280,000	\$735,000	n/a	n/a
Utility/Railroad Cost	\$50,000	\$90,000	\$65,000	\$85,000	\$70,000	n/a	n/a
Total	\$6,586,000	\$7,441,000	\$7,410,000	\$8,806,000	\$7,450,000	\$6,593,000	n/a
Design Concerns	Substantial Right of Way Involvement Potential for Hazardous Waste Natural Resource Concerns Historical Impacts	Moderate Right of Way involvement Natural Resource Concerns Historical Impacts	Right of Way Involvement Natural Resource Concerns Historical Impacts	Right of Way Involvement Potential for Hazardous Waste Natural Resource Concerns Historical Impacts	Right of Way Involvement Natural Resource Concerns Historical Impacts	After Rehabilitation the bridge would be classified as functionaly obsolete under FHWA criteria due to the 20'0" roadway width.	The no build option would eventually lead to the closure of the bridge at sometime in the future. A feasible existing detour route does not exist. This is a dead end route to which there is no other access that would permit closure of CR 12/4 at this site.

Public Roads Div.	State Dist. No.	State Project No.	Federal Project No.	Fiscal Year	County	Sheet No.	Total Sheets
W.V.	2	S323-12/4-0.05	BR-0124 (003)D	2012	LOGAN		

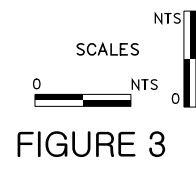


NOTE:
 ALTERNATIVE 1 PROPOSES REPLACING THE BRIDGE ON A NEW ALIGNMENT APPROXIMATELY 475 FEET (CENTERLINE TO CENTERLINE) UPSTREAM FROM THE EXISTING BRIDGE. EXISTING BRIDGE TO PROVIDED M.O.T. DURING CONSTRUCTION.

NOTE:
 PROPERTY INFORMATION FROM GUYAN (HENLAWSON) DISTRICT - TAX MAPS.
 WV 10 RIGHT-OF-WAY INFORMATION FROM PROJECT NO. 3290
 CR 12/4 RIGHT-OF-WAY INFORMATION FROM PROJECT NO. 6518 (1952)
 RAILROAD RIGHT-OF-WAY INFORMATION FROM VALUATION MAP V16062 (1948)



NOTE:
 VERTICAL SCALE EXAGGERATED 2:1

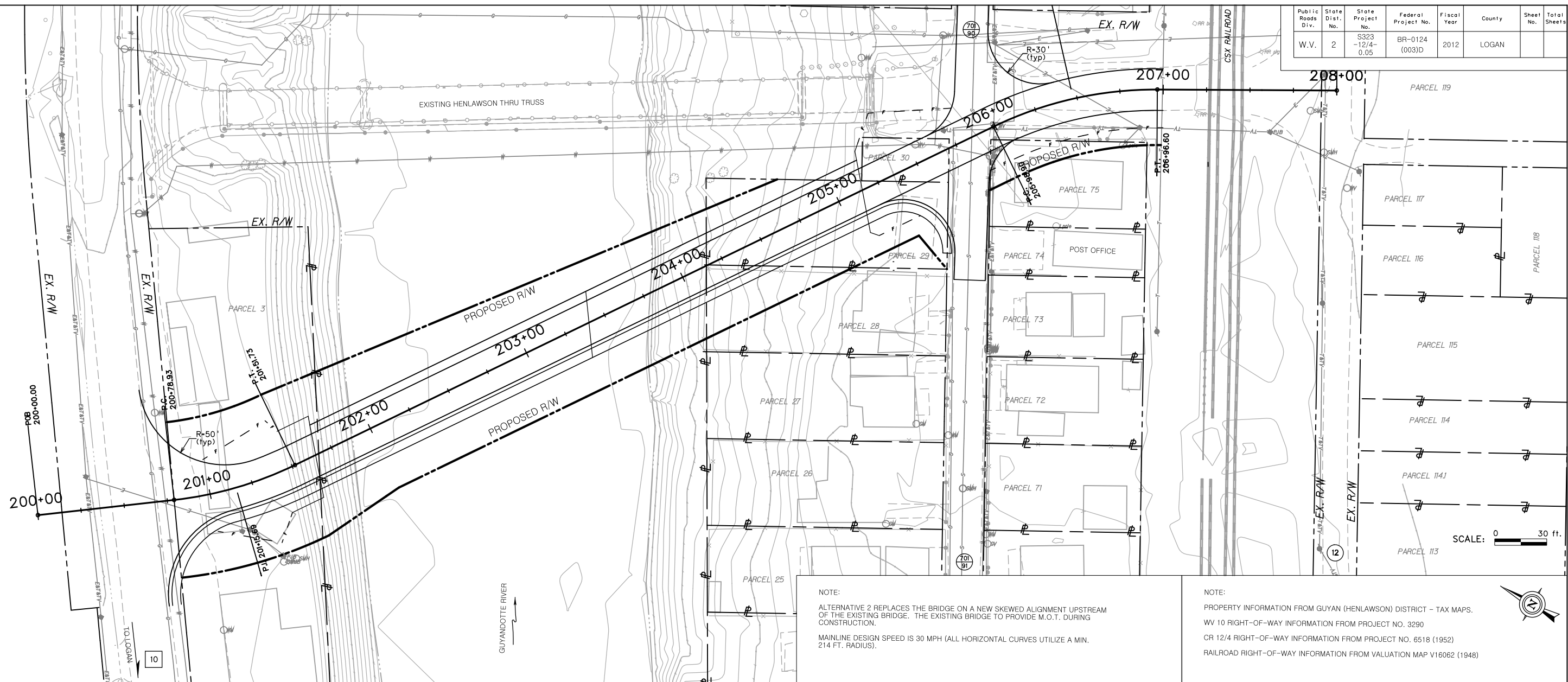


REVISION NUMBER	SHEET NUMBER	REVISION	DATE	BY

THE WEST VIRGINIA DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS

ALTERNATIVE 1 PLAN & PROFILE SHEET

Public Roads Div.	State Dist. No.	State Project No.	Federal Project No.	Fiscal Year	County	Sheet No.	Total Sheets
W.V.	2	S323-12/4-0.05	BR-0124 (003)D	2012	LOGAN		



NOTE:
 ALTERNATIVE 2 REPLACES THE BRIDGE ON A NEW SKEWED ALIGNMENT UPSTREAM OF THE EXISTING BRIDGE. THE EXISTING BRIDGE TO PROVIDE M.O.T. DURING CONSTRUCTION.
 MAINLINE DESIGN SPEED IS 30 MPH (ALL HORIZONTAL CURVES UTILIZE A MIN. 214 FT. RADIUS).

NOTE:
 PROPERTY INFORMATION FROM GUYAN (HENLAWSON) DISTRICT - TAX MAPS.
 WV 10 RIGHT-OF-WAY INFORMATION FROM PROJECT NO. 3290
 CR 12/4 RIGHT-OF-WAY INFORMATION FROM PROJECT NO. 6518 (1952)
 RAILROAD RIGHT-OF-WAY INFORMATION FROM VALUATION MAP V16062 (1948)

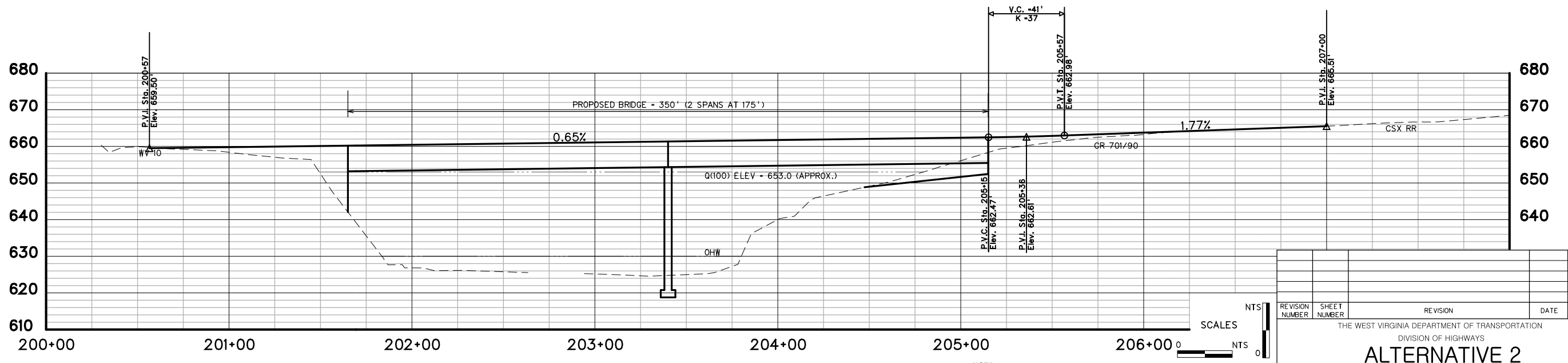
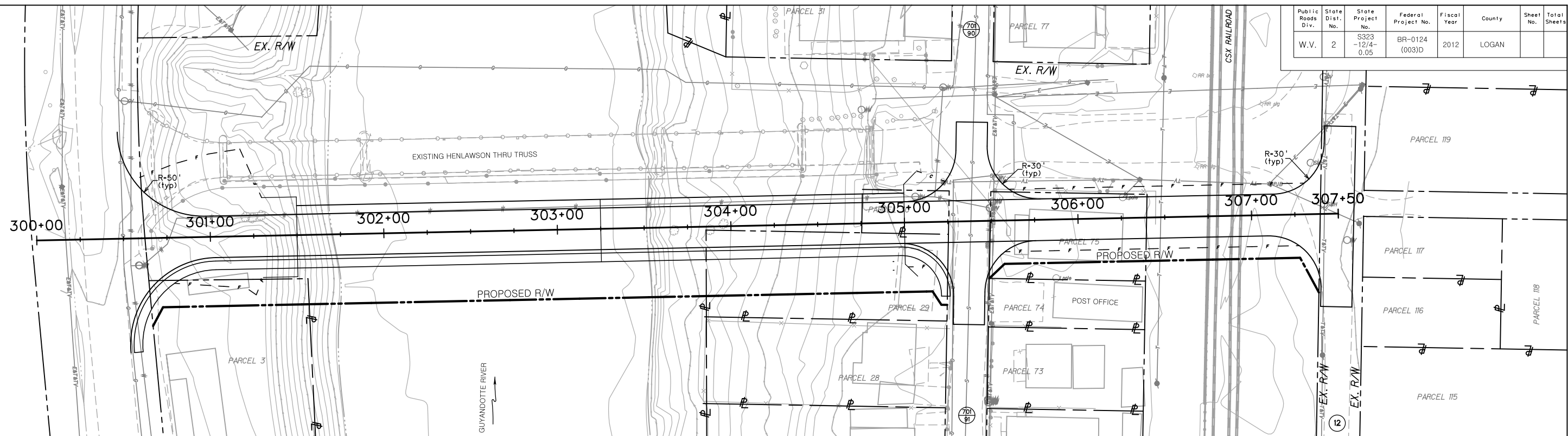


FIGURE 4
 VERTICAL SCALE EXAGGERATED 2:1

REVISION NUMBER	SHEET NUMBER	REVISION	DATE	BY

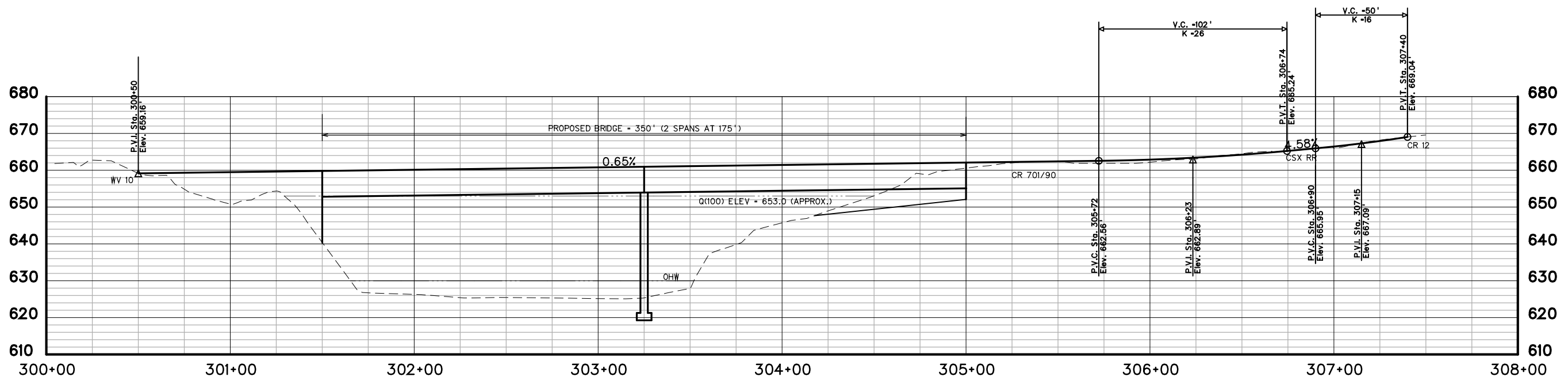
THE WEST VIRGINIA DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS
ALTERNATIVE 2
PLAN & PROFILE SHEET

Public Roads Div.	State Dist. No.	State Project No.	Federal Project No.	Fiscal Year	County	Sheet No.	Total Sheets
W.V.	2	S323-12/4-0.05	BR-0124 (003)D	2012	LOGAN		

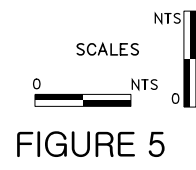


NOTE:
 ALTERNATIVE 3 REPLACES THE BRIDGE ON A NEW ALIGNMENT APPROXIMATELY 44 FEET (CENTERLINE TO CENTERLINE) UPSTREAM FROM THE EXISTING BRIDGE. EXISTING BRIDGE TO PROVIDE M.O.T. DURING CONSTRUCTION.

NOTE:
 PROPERTY INFORMATION FROM GUYAN (HENLAWSON) DISTRICT - TAX MAPS.
 WV 10 RIGHT-OF-WAY INFORMATION FROM PROJECT NO. 3290
 CR 12/4 RIGHT-OF-WAY INFORMATION FROM PROJECT NO. 6518 (1952)
 RAILROAD RIGHT-OF-WAY INFORMATION FROM VALUATION MAP V16062 (1948)



NOTE:
 VERTICAL SCALE EXAGGERATED 2:1

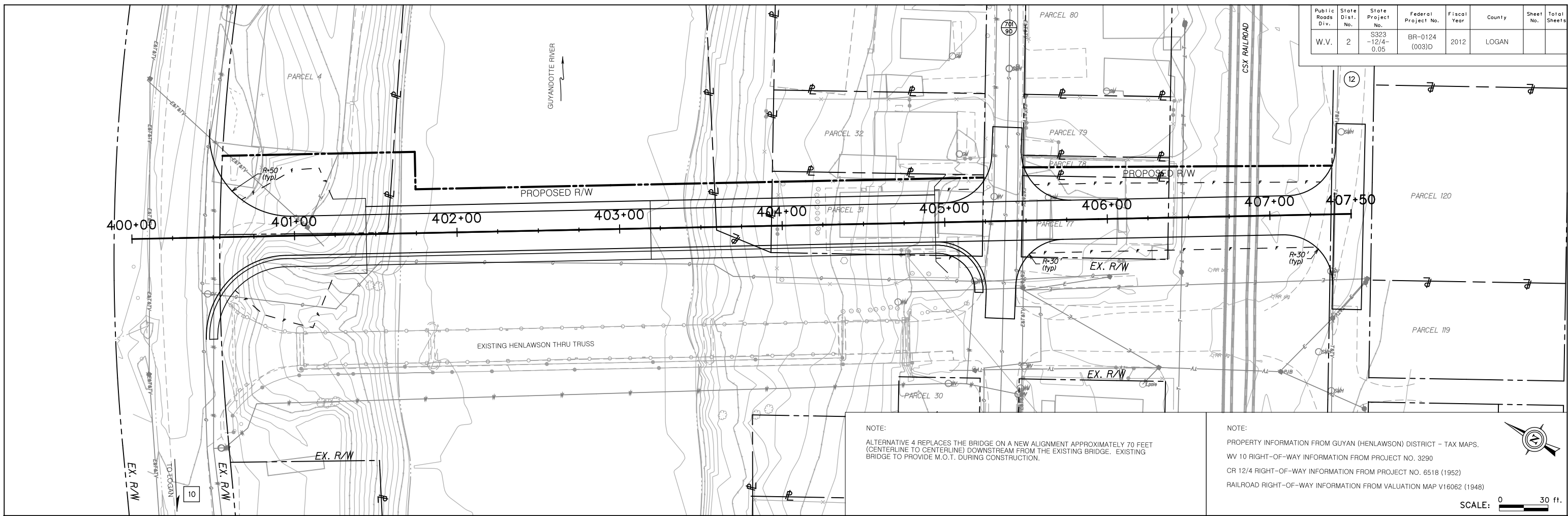


REVISION NUMBER	SHEET NUMBER	REVISION	DATE	BY

THE WEST VIRGINIA DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS

ALTERNATIVE 3 PLAN & PROFILE SHEET

Public Roads Div.	State Dist. No.	State Project No.	Federal Project No.	Fiscal Year	County	Sheet No.	Total Sheets
W.V.	2	S323-12/4-0.05	BR-0124 (003)D	2012	LOGAN		

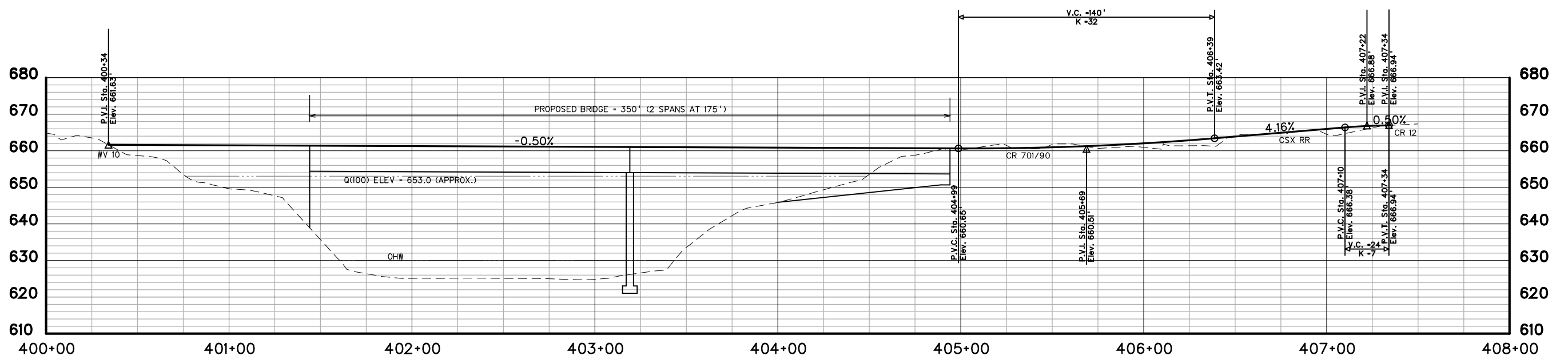


NOTE:
 ALTERNATIVE 4 REPLACES THE BRIDGE ON A NEW ALIGNMENT APPROXIMATELY 70 FEET (CENTERLINE TO CENTERLINE) DOWNSTREAM FROM THE EXISTING BRIDGE. EXISTING BRIDGE TO PROVIDE M.O.T. DURING CONSTRUCTION.

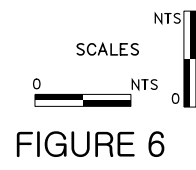
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 PROPERTY INFORMATION FROM GUYAN (HENLAWSON) DISTRICT - TAX MAPS.
 WV 10 RIGHT-OF-WAY INFORMATION FROM PROJECT NO. 3290
 CR 12/4 RIGHT-OF-WAY INFORMATION FROM PROJECT NO. 6518 (1952)
 RAILROAD RIGHT-OF-WAY INFORMATION FROM VALUATION MAP V16062 (1948)



SCALE: 0 30 ft.



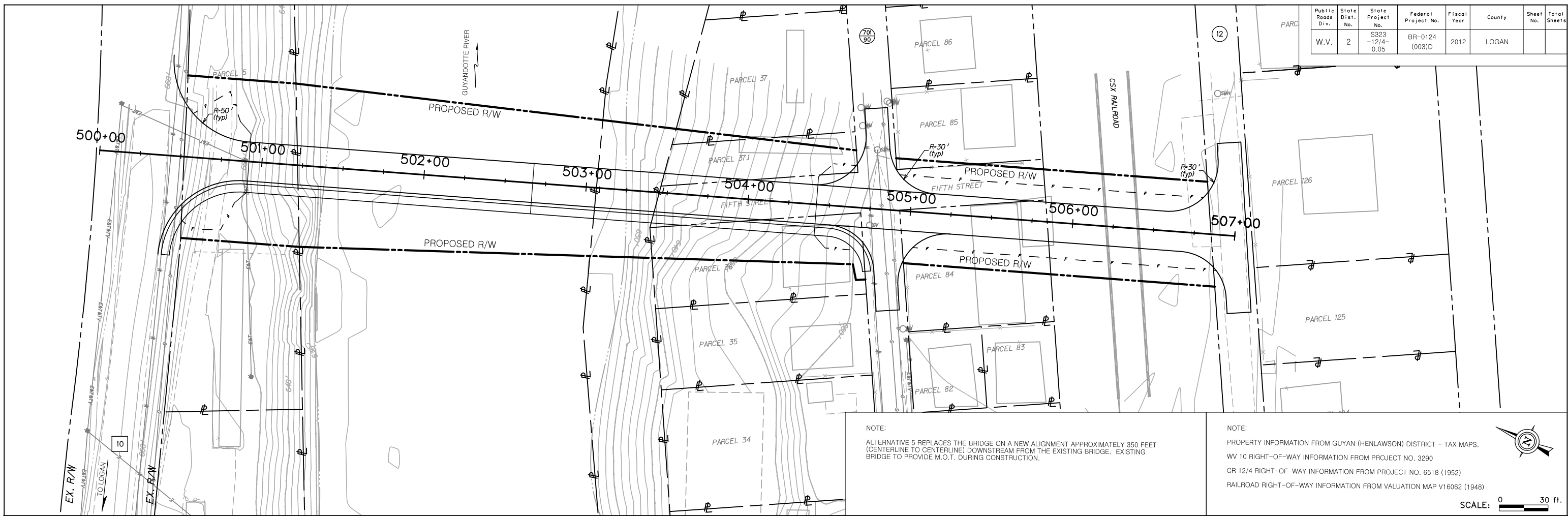
NOTE:
 VERTICAL SCALE EXAGGERATED 2:1



REVISION NUMBER	SHEET NUMBER	REVISION	DATE	BY

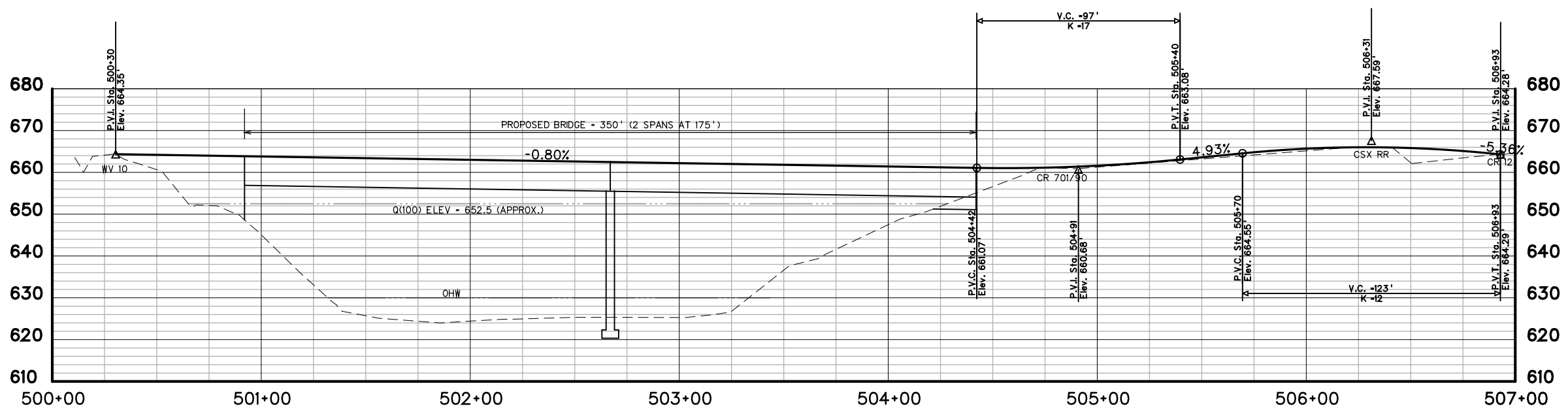
THE WEST VIRGINIA DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS
**ALTERNATIVE 4
 PLAN & PROFILE SHEET**

Public Roads Div.	State Dist. No.	State Project No.	Federal Project No.	Fiscal Year	County	Sheet No.	Total Sheets
W.V.	2	S323-12/4-0.05	BR-0124 (003)D	2012	LOGAN		

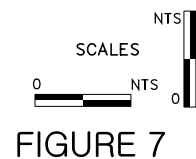


NOTE:
 ALTERNATIVE 5 REPLACES THE BRIDGE ON A NEW ALIGNMENT APPROXIMATELY 350 FEET (CENTERLINE TO CENTERLINE) DOWNSTREAM FROM THE EXISTING BRIDGE. EXISTING BRIDGE TO PROVIDE M.O.T. DURING CONSTRUCTION.

NOTE:
 PROPERTY INFORMATION FROM GUYAN (HENLAWSON) DISTRICT - TAX MAPS.
 WV 10 RIGHT-OF-WAY INFORMATION FROM PROJECT NO. 3290
 CR 12/4 RIGHT-OF-WAY INFORMATION FROM PROJECT NO. 6518 (1952)
 RAILROAD RIGHT-OF-WAY INFORMATION FROM VALUATION MAP V16062 (1948)



NOTE:
 VERTICAL SCALE EXAGGERATED 2:1



REVISION NUMBER	SHEET NUMBER	REVISION	DATE	BY

THE WEST VIRGINIA DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS

ALTERNATIVE 5 PLAN & PROFILE SHEET

DATE:

Mr. Gregory L. Bailey, P.E.
Director, Engineering Division
West Virginia Division of Highways
State Capitol Complex, Building 5
1900 Kanawha Boulevard East
Charleston, West Virginia 25305-0430

DATE: Tuesday, March 6, 2012
LOCATION: WV Department of Veterans Assistance Field Office
(Old Henlawson Elementary School)
SUBJECT: INFORMATIONAL WORKSHOP PUBLIC MEETING
PROJECT: Henlawson Thru Truss Bridge Replacement
Logan County

COMMENTS DUE BY Friday, April 6, 2012

Please consider the following comments:

(Please print the following information)

NAME:

ADDRESS:

ORGANIZATION (IF ANY):

How did you hear about the Informational Workshop Public Meeting?

Project Information and Comment Sheets
Can be found online at our web page www.transportation.wv.gov.
Click on Comment on projects, Open, and then click on Henlawson Thru Truss Bridge
Project.